

ALLEN COUNTY REGIONAL AIRPORT AUTHORITY
ALLEN COUNTY AIRPORT
LIMA, OHIO

APRON REHAB PAVEMENT SURFACE (188,000 SQ FT.)
SEAL RUNWAY PAVEMENT SURFACE (900,000 SQ FT.)
AIP No. 3-39-046-023-2021

ALLEN COUNTY
REGIONAL AIRPORT AUTHORITY

APPROVED: _____
JOHN PISLE, PRESIDENT
DATE: _____

APPROVED: _____
ERIC DAVIS, MEMBER
DATE: _____

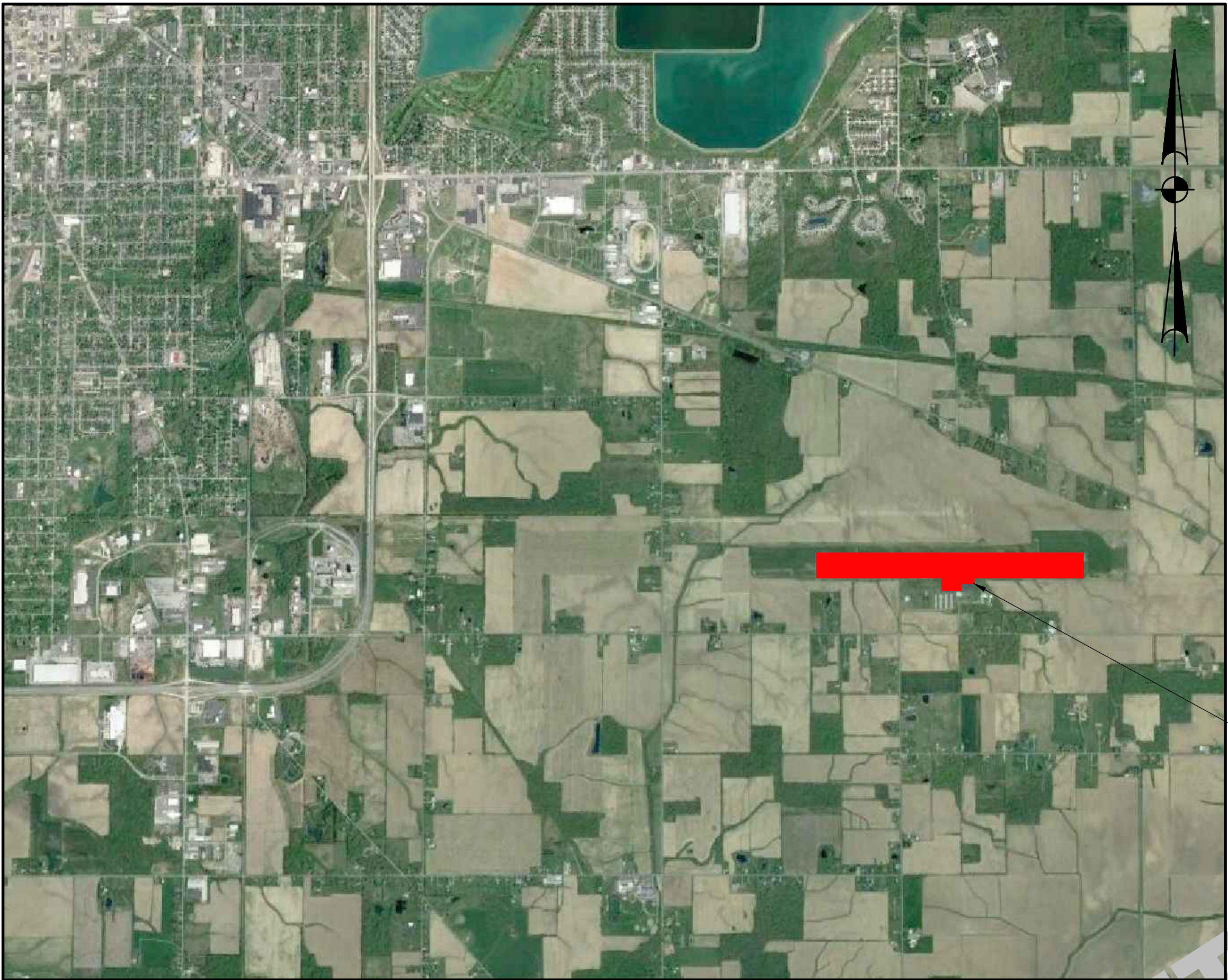
APPROVED: _____
TRACIE SANCHEZ, MEMBER
DATE: _____

APPROVED: _____
JON NEUMAN, MEMBER
DATE: _____

APPROVED: _____
DAVID BUETTNER, MEMBER
DATE: _____

APPROVED: _____
LARRY WEBB, MEMBER
DATE: _____

APPROVED: _____
DEREK YOUNKMAN, MEMBER
DATE: _____



VICINITY MAP
NOT TO SCALE

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REVISIONS			
NO.	REMARK	DATE	BY
1			
2			
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RECOMMENDED
FOR APPROVAL: _____
DESIGN ENGINEER
DATE: 03/24/2021

DESIGNED: BSE DRAWN: BSE
CHECKED: JRC CHECKED: JRC

ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL
TITLE AND INDEX SHEET
LIMA OHIO

Headquarters
8450 WESTFIELD BLVD., SUITE 300
INDIANAPOLIS, IN 46240-8302
TEL 317-713-4615
FAX 317-713-4616
www.BFSengr.com

BFS
Butler Fairman Seufert
ENGINEERS

Branch Locations
FORT WAYNE 260-459-1532
LOUISVILLE 502-593-1996
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

HORIZONTAL SCALE
VERTICAL SCALE
N/A

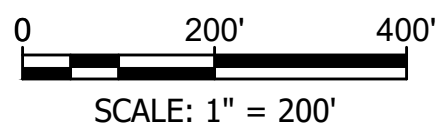
SHEET
01 OF 11
PROJECT
AIP No. 3-39-046-023-2021

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NOTES:

1. THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF THE WORK ITEMS AND THEIR APPROXIMATE LOCATIONS AND LIMITS FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. IT SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE PLAN SHEETS WHICH FOLLOW FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
2. THIS PROJECT CONSISTS OF REPLACEMENT OF THE RUNWAY 28 PAPI UNITS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE FOLLOWING PLANS, INCLUDING BUT NOT LIMITED TO:
 - MILL AND OVERLAY ASPHALT (APRON)
 - REMOVE RUNWAY MARKINGS
 - SEAL COAT RUNWAY
 - APPLY PAVEMENT MARKINGS TO APRON AND RUNWAY
3. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED, AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT, TO THE SATISFACTION OF THE OWNER, SHALL BE INCLUDED IN THE COST OF PERFORMING THESE PAY ITEMS.
4. THE CONTRACTOR SHALL GIVE CONSTANT ATTENTION TO THE WORK TO FACILITATE THE PROGRESS THEREOF AND SHALL COORDINATE WITH THE OWNER IN EVERY WAY POSSIBLE. THE OWNER SHALL ALLOCATE THE WORK AND DESIGNATE THE SEQUENCE OF CONSTRUCTION IN CASE OF CONTROVERSY BETWEEN THE CONTRACTOR AND AIRPORT.
5. THE CONTRACTOR SHALL PAY CLOSE ATTENTION TO THE SAFETY AND PHASING PLANS AND THE AIRPORT SAFETY REQUIREMENTS SECTION OF THE SPECIFICATIONS. THESE SHALL BE STRICTLY ENFORCED.
6. CONSTRUCTION WILL BE IN OR ADJACENT TO RUNWAY AND TAXIWAY OPERATIONAL AREAS. ALL RUBBISH AND DEBRIS RESULTING FROM THE WORK SHALL BE REMOVED FROM THE SITE BY THE CONTRACTOR ON A CONTINUAL BASIS.
7. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL REMOVE FROM THE SITE ALL SURPLUS MATERIAL AND EQUIPMENT BELONGING TO THE CONTRACTOR AND HIS/HER SUBCONTRACTORS.
8. THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION MEETING PRIOR TO BEGINNING WORK TO COORDINATE WORK PROCEDURES WITH ALL INTERESTED PARTIES. THE CONTRACTOR SHALL PROVIDE A DETAILED CONSTRUCTION SCHEDULE TO THE OWNER A MINIMUM OF 7 DAYS PRIOR TO THE PRE-CONSTRUCTION MEETING FOR APPROVAL. THE CONTRACTOR SHALL ALSO ATTEND ANY JOB MEETING CALLED BY THE ENGINEER.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE IMMEDIATE CORRECTION TO ALL DAMAGE TO AIRPORT PROPERTY CAUSED BY HIS/HER FORCES OR SUBCONTRACTORS.

10. THE CONTRACTOR SHALL SUPPLY ONE SET OF AS-BUILT DRAWINGS TO THE ENGINEER AT THE COMPLETION OF THE PROJECT. THE CONTRACTOR SHALL KEEP THE RED LINE DRAWING SET ON SITE AND SHALL UPDATE ON A DAILY BASIS THROUGHOUT THE PROJECT.
11. CONTRACTOR SHALL USE HAUL ROUTE AS SHOWN. CONTRACTOR SHALL YIELD TO ALL AIRCRAFT ACTIVITIES AT ALL TIMES. CONTRACTOR SHALL NOT PARK ANY EQUIPMENT AND/OR PERSONAL VEHICLES IN FRONT OF ANY HANGARS AND ON OR NEAR ANY TIE DOWN MARKINGS.
12. CONTRACTOR SHALL PLACE STONE FOR HAUL ROUTE IF NEEDED. AT THE END OF CONSTRUCTION, CONTRACTOR SHALL REPAIR GROUND BACK TO ITS ORIGINAL CONDITION. COST FOR STONE SHALL BE INCLUDED IN MAINTENANCE OF TRAFFIC COST ITEM.
13. CONTRACTOR SHALL PROTECT EXISTING PAINT MARKINGS DURING CONSTRUCTION. IF ANY DAMAGE TO EXISTING PAINT MARKINGS, CONTRACTOR SHALL REPAINT AT HIS OR HER OWN EXPENSE.

AIRPORT CONTACTS

AIRPORT MANAGER:
JOSH TATTRIE
(567) 208-1871

LEGEND



HAUL ROUTE



STAGING AND STOCKPILE AREA



RESTRICTED FROM CONSTRUCTION HAUL ROUTE TRAFFIC

REVISIONS			
NO.	REMARK	DATE	BY
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RECOMMENDED FOR APPROVAL:	DESIGN ENGINEER	DATE
DESIGNED:	BSE	DRAWN: BSE
CHECKED:	JRC	CHECKED: JRC

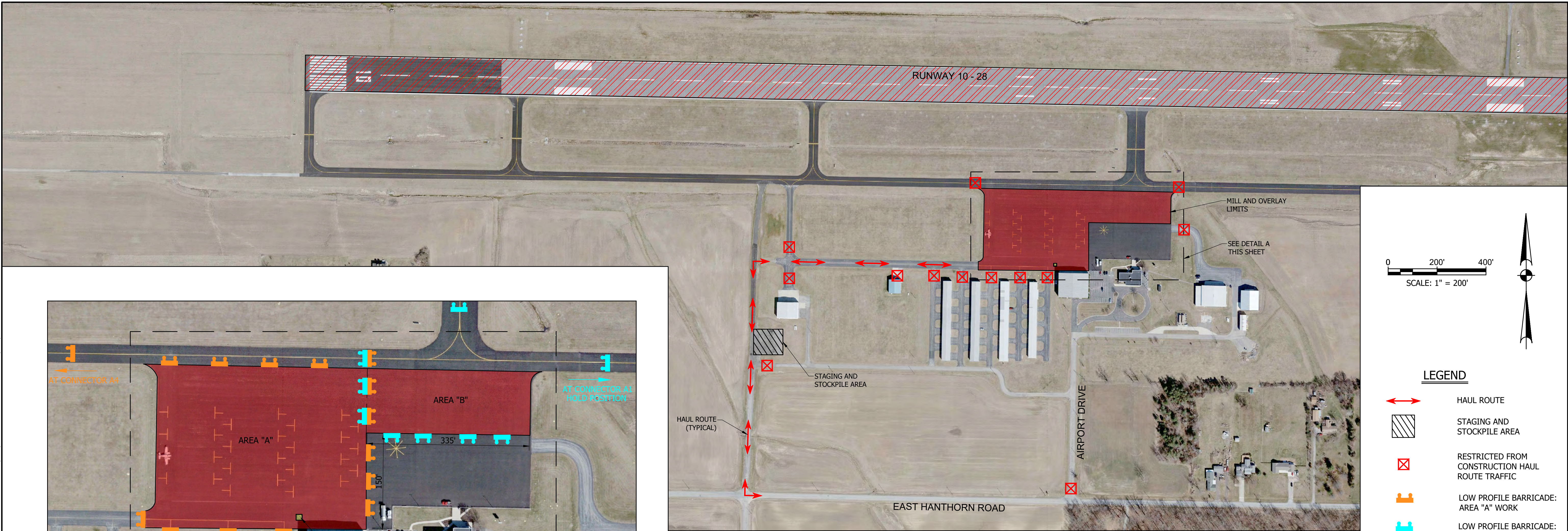
ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL	
SCOPE OF WORK	
LIMA	OHIO
Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSengr.com	
BFS Butler Fairman Seufert ENGINEERS	
Branch Locations FORT WAYNE 260-459-1532 LOUISVILLE 502-593-1996 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242	

HORIZONTAL SCALE	
1" = 200'	
VERTICAL SCALE	
N/A	
SHEET	
02	OF 11
PROJECT	
AIP No. 3-39-046-023-2021	

BFS NO. 6520.9802



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DETAIL A

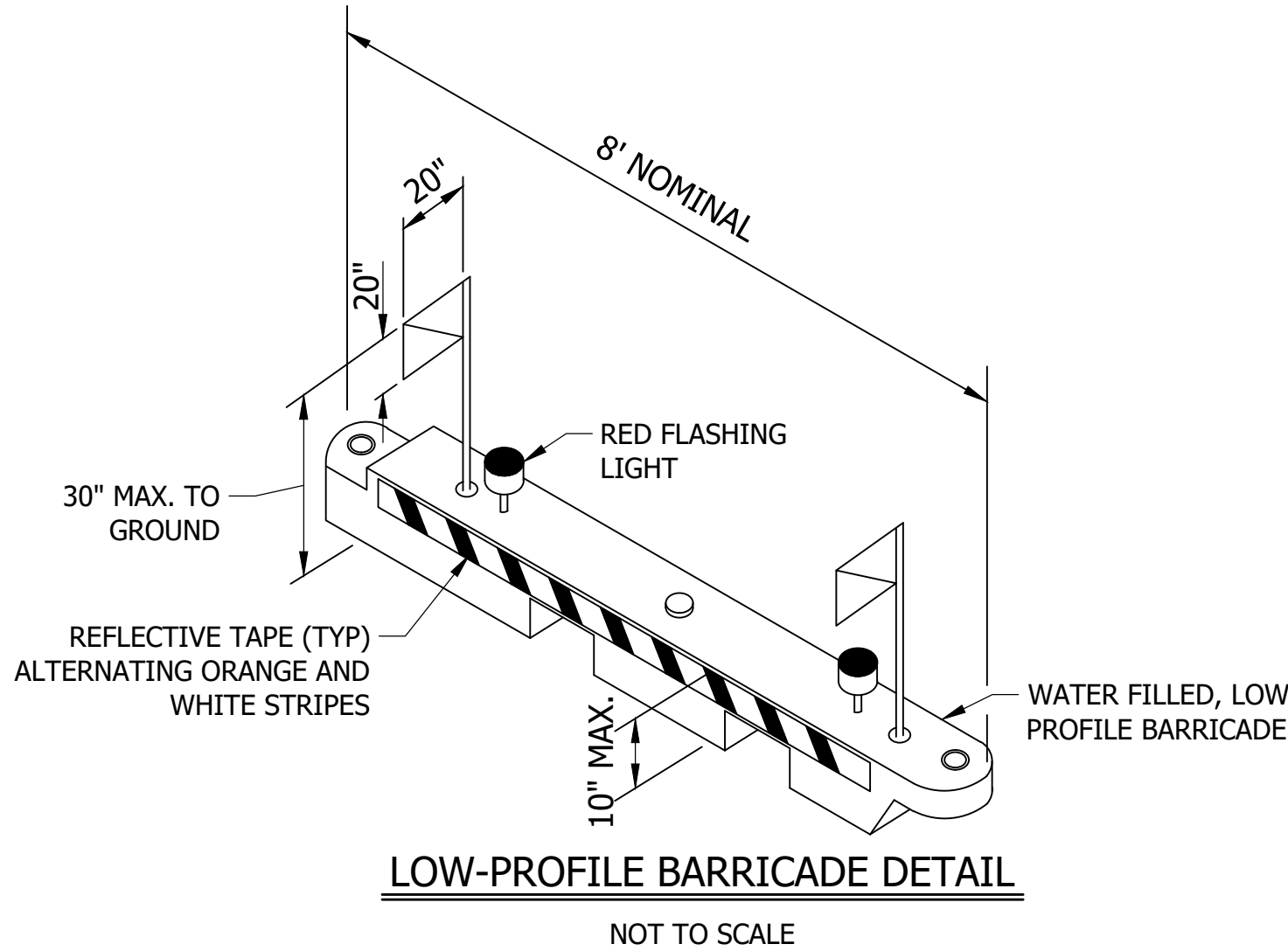
Scale: 1" = 100'

GENERAL NOTES:

- CONTRACTOR MUST MAINTAIN ACCESS AT ALL TIMES FOR AIRCRAFT AND EQUIPMENT TO THE APRON AREA IMMEDIATELY IN FRONT OF THE TERMINAL BUILDING. ALTERATIONS TO THE DIVIDING LINE BETWEEN AREA "A" AND AREA "B" MAY BE SUBMITTED BY THE CONTRACTOR FOR REVIEW BY THE ENGINEER AND OWNER. ALL EFFORTS AND MATERIALS TO MAINTAIN ACCESS SHALL BE INCIDENTAL TO THE MAINTENANCE OF TRAFFIC PAY ITEM.
- CONTRACTOR SHALL MAINTAIN DRAINAGE AT ALL TIMES. RESTORE TO ORIGINAL CONDITION ALL DISTURBED AREAS UPON COMPLETION OF PROJECT.
- CONTRACTOR SHALL PROTECT EXISTING PAINT MARKINGS AT ALL TIMES. IF ANY MARKINGS ARE DAMAGED, CONTRACTOR SHALL REPAINT AT HIS/HER OWN EXPENSE.
- PRIOR TO REOPENING A SITE INSPECTION SHALL BE CONDUCTED BY THE AIRPORT TO ENSURE THAT ALL AIRPORT SURFACES ARE CLEAN OF ANY DEBRIS.
- CONTRACTOR SHALL MILL ASPHALT FROM EXISTING APRON AND THE MILLINGS SHALL BE REMOVED OFFSITE AND PROPERLY DISPOSED OFF.
- KEEP APRON PORTIONS ONLY USED FOR HAULING CLEAN AT ALL TIMES, EVEN DURING HAULING OPERATIONS TO AND FROM SITE.
- HAVE VAC TRUCK AVAILABLE AT ALL TIMES DURING OPERATIONS REQUIRING CONSTRUCTION TRAFFIC TO HAUL TO AND FROM SITE TO CLEAN FOREIGN OBJECT DEBRIS (FOD) FROM AIRCRAFT PAVEMENT IMMEDIATELY.
- CONTRACTOR SHALL BE RESPONSIBLE FOR IDENTIFYING ALL BELOW GROUND UTILITIES, INCLUDING AIRPORT OWNED, WITHIN CONSTRUCTION LIMITS AND ALL HAUL ROUTES.
- CONTRACTOR SHALL USE HAUL ROUTE AS SHOWN. CONTRACTOR SHALL YIELD TO ALL AIRCRAFT ACTIVITIES AT ALL TIMES. CONTRACTOR SHALL NOT PARK ANY EQUIPMENT AND/OR PERSONAL VEHICLES IN FRONT OF ANY HANGARS AND ON OR NEAR ANY TIE DOWN MARKINGS.

BARRICADE NOTES

- CONTRACTOR SUPPLIED BARRICADES SHALL BE PLACED 10' MAXIMUM SEPARATION; OR AS INSTRUCTED BY THE ENGINEER OR OWNER.
- INSTALL WHERE INDICATED ON CONSTRUCTION SAFETY PLAN OR IN LOCATIONS APPROVED BY THE ENGINEER OR OWNER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPLYING, PLACING, MOVING, LIGHTING AND MAINTAINING THE BARRICADES DURING CONSTRUCTION OF THE ENTIRE PROJECT.
- THE BARRICADES SHALL BE WEIGHTED AND CAPABLE OF WITHSTANDING UP TO 110 M.P.H. WIND FORCES.
- INSTALL A MINIMUM OF 2 BATTERY OPERATED RED PHOTOCELL LIGHTS EQUALLY SPACED PER EACH 8' LONG BARRICADE. LIGHTS SHALL MAINTAIN SUCH INTENSITY SO AS TO BE READILY IDENTIFIED FROM DISTANCES OF 200' OR GREATER DURING DARKNESS PERIODS.
- A MINIMUM OF 2 FLAGS NO MORE THAN 20" SQUARE MOUNTED TO THE BARRICADE AND NO MORE THAN 30" HIGH SHALL BE AFFIXED FOR EACH SPAN OF BARRICADES. FLAGS SHALL BE AVIATION ORANGE (FED-STD-595, NO. 12197).
- THE COST OF PROVIDING AND MANIPULATING BARRICADES SHALL BE INCIDENTAL TO MAINTENANCE OF TRAFFIC AND SHALL NOT BE A PAY ITEM.



LOW-PROFILE BARRICADE DETAIL

NOT TO SCALE

REVISIONS			
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RECOMMENDED FOR APPROVAL:	DATE
DESIGNED: BSE	DRAWN: BSE
CHECKED: JRC	CHECKED: JRC

ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL
CONSTRUCTION SAFETY PHASING PLAN PHASE 2
LIMA OHIO

Headquarters
8450 WESTFIELD BLVD., SUITE 300
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Branch Locations
FORT WAYNE 260-459-1532
LOUISVILLE 502-593-1996
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-639-3242

HORIZONTAL SCALE	
1" = 200'	
VERTICAL SCALE	
N/A	
SHEET	
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PROJECT	
AIP No. 3-39-046-023-2021	

BFS NO. 6520.9802

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Construction Safety and Phasing Plan (CSPP):

Scope of Work: This project at the Allen County Airport consists of work on the aircraft parking apron and the runway, including but not limited to:

- Remove runway pavement markings
- Repair cracks in asphalt runway
- Apply runway Asphalt Seal Coat
- Replace runway pavement markings
- Mill aircraft apron asphalt surface (20,830 sq. yds.)
- Repair cracks in milled surface
- Place asphalt overlay
- Replace apron pavement markings

General: The CSPP has been developed utilizing the process set forth in AC 150/5370-2F "Operational Safety on Airports During Construction". Everyone has a role in operational safety on airports during construction: the airport operator, the airport's consultants, the construction contractor and subcontractors, airport users, airport tenants, ARFF personnel, Air Traffic personnel, including Technical Operations personnel, FAA Airports Division personnel, and others. Close communication and coordination between all affected parties is the key to maintaining safe operations. Such communication and coordination should start at the project scoping meeting and continue through the completion of the project. The airport operator and contractor should conduct onsite safety inspections throughout the project and immediately remedy any deficiencies, whether caused by negligence, oversight, or project scope change.

SPCID: The contractor is responsible for submitting a Safety Plan Compliance Document (SPCD) which details how the contractor will comply with the CSPP. It is not possible to determine all safety plan details, which are specific to the contractor, during the development of the CSPP. The successful contractor will develop a SPCD that includes equipment hazards, contractor's points of contact, equipment heights, etc. that will be submitted to the airport for review prior to the issuance of a notice-to-proceed. A sample of a SPCD is provided in the project specifications.

(1) Coordination

Airport Operators conducting construction will use pre-design, pre-bid and pre-construction conferences to introduce the subject of airport operational safety during construction (see AC 150/5300-9). In addition, the following should be coordinated as required:

- a.) **Contractor Progress Meetings:** Operational safety will be a standing agenda item for discussion during progress meetings throughout the project. The progress meetings will be held either weekly or bi-weekly at the owner's and engineer's discretion and the frequency may vary during the course of the project. Attendance by the prime contractor and relevant sub-contractors is mandatory.
- b.) **Scope or Schedule Changes:** Changes in the scope or duration of the project may necessitate revisions to the CSPP and review and approval by the airport operator and the FAA. The prime contractor will submit a schedule at the beginning of the project and will be required to update the schedule as deviations occur due to weather, unforeseen circumstances, etc.
- c.) **FAA ATO Coordination:** Early coordination with FAA ATO is required to schedule airway facility shutdowns and restarts. Relocation or adjustments to NAVAIDs, or changes to final grades in critical areas, may require an FAA flight inspection prior to restarting the facility. Flight inspections must be coordinated and scheduled well in advance of the intended facility restart.

(2) Phasing

The project will be completed in a two phases and is outlined below:

Phase	General Scope	Calendar Days	Scheduled Date(s)	Work Hour Restrictions
1A	Remove Runway Markings, Seal Coat Runway, Temp. Markings	7	TBD	No Work Hour Restrictions
1B	Runway Permanent Markings	2	TBD (Approx. 28 Days After Completion of Phase 1A)	No Work Hour Restrictions
2A	Mill Asphalt Surface, Place New Asphalt Surface	12	TBD	No Work Hour Restrictions
2B	Apron Pavement Markings	1	TBD (Approx. 28 Days After Completion of Phase 2A)	No Work Hour Restrictions

- (3) Areas and Operations Affected by the Construction Activity
- See Operations Effects table below.

Table 1: Airport Operations Affected by Construction

Phase	Taxiway Restrictions	Runway Restrictions
1A	No Restrictions	Closed
1B	No Restrictions	Closed During Working Hours Only
2A - Area "A"	Taxiway A Closed Between A-4 and A-3	No Restrictions
2A - Area "B"	Taxiway A Closed Between A-1 and A-3	No Restrictions
2B	No Restrictions	No Restrictions

Table 2: Safety Area of Active Runways

Runway	Normal	Phase 1	Runway Safety Area Width / 2
10-28	D	D	250'

Table 3: Runway Approach Protection Areas During Construction

Runway End	Aircraft Approach Category	Airplane Design Group	Safety Area Prior to Threshold	Minimum Distance to Threshold on Approach Slope
10	D	III	1,000'	1,000' 20:1
28	D	III	1,000'	1,000' 50:1

*See CSPP plan sheets for graphical representation of impacts.

(4) Protection of Navigational Aids (NAVAIDS)

Before commencing construction activity, parking vehicles or storing construction equipment and materials near a NAVAID, coordinate with the appropriate FAA ATO/Technical Operations Office to evaluate the effect of construction activity and the required distance and direction from the NAVAID. Construction activities, material/equipment storage, and vehicle parking near electronic NAVAIDS require special consideration since they may interfere with signals essential to air navigation. This project will require no impacts to NAVAIDS.

Table 4: NAVAID Facility Impacts

Facility Type	Phase Impacted	Impact
Runway 28 Glideslope	None	None
Runway 28 Localizer	None	None
Runway 28 MALSR	None	None
Runway 28 REIL	Phase 1	Out of Service
Runway 28 PAPI	Phase 1	Out of Service
Runway 10 REIL	Phase 1	Out of Service
Runway 10 PAPI	Phase 1	Out of Service

(5) Contractor Access

- a.) **Location of Stockpiled Materials:** Stockpiled materials and equipment storage are not permitted within the Runway or Taxiway Safety Area (RSA/ TSA) and Object Free Zone (OFZ), and if possible should not be permitted within the Object Free Area (OFA) of an operational runway. No stockpiles will be created by the project.

- b.) **Vehicle and Pedestrian Operations:** Vehicle and pedestrian access routes for airport construction projects must be controlled to prevent inadvertent or unauthorized entry of persons, vehicles, or animals onto the Aircraft Operations Area (AOA). The airport operator should coordinate requirements for vehicle operations with airport tenants, contractors, and the FAA air traffic manager.
- 1.) **Construction Site Parking:** The area for vehicle parking for contractor employees is shown on the plan sheets of the CSPP. There shall be no unauthorized entry of persons or vehicles onto the AOA.

- 2.) **Construction Equipment Parking:** Contractor employees must park and service all construction vehicles in the designated staging area and never in the safety area of an active runway or taxiway. Unless a complex setup procedure makes movement of specialized equipment infeasible, inactive equipment must not be parked on a closed taxiway or runway. If it is necessary to leave specialized equipment on a closed taxiway or runway at night, the equipment must be well lighted. A maximum equipment height of 25 feet will be enforced, with the location submitted on a FAA Form 7460-1.

- 3.) **Access and Haul Roads:** The access and haul road to the project site will be from multiple points due to the varied locations of the work areas as shown on the plan sheets of CSPP. The contractor is not permitted to use any other access points. This access shall be clearly marked by the contractor to prevent trucks and personnel from inadvertently entering into area open to airport operations. The maximum equipment height on the access and haul road will be 25 feet.

- 4.) **Marking and Lighting of Vehicles:** Per AC 150/5210-5, the standard for identification lighting is a yellow flashing light that is mounted on the uppermost part of the vehicle structure. The light must be visible from any direction, day and night, including from the air. For vehicles and/or construction equipment where a light is not feasible, a flag must be attached that is readily visible. The flag must be at least a 3-foot by 3-foot square having a checkered pattern of international orange and white squares with at least 1 foot on each side.

- 5.) **Description of Proper Vehicle Operations:** All contractor vehicles shall be in proper and safe working order. Under normal conditions vehicles shall follow two-way radio communications procedures outlined below in 9.), under lost communications or emergency conditions all vehicles shall return to the staging area immediately while avoiding the AOA and all safety areas.

- 6.) **Required Escorts:** Escorts are not required for this project unless the contractor cannot provide adequate personnel to utilize two-way radio communications for their and their subcontractors' work efforts. Neither the airport operator, nor the Engineer shall be responsible for escorting the contractor.

- 7.) **Training Requirement for Vehicle Drivers:** There is no formal driver training course currently at Allen County Airport. It is not anticipated that this project would require the contractor to access the AOA. If situations arise, the contractor should contact the Engineer or Airport Operator. The Airport Operator and Engineer reserve the right to revoke driving privileges from contractor personnel. All personnel driving on the airport shall be familiar with the FAA publication "FAA Guide to Ground Vehicle Operations." The airport operator and the engineer also reserve the right to revoke driving privileges from contractor personnel.

- 8.) **Situational Awareness:** Vehicle drivers must confirm by personal observation that no aircraft is approaching their position (either in the air or on the ground) when given clearance to cross a runway, taxiway, or any other area open to airport operations. In addition, it is the responsibility of the escort vehicle driver to verify the movement/position of all escorted vehicles at any given time.

- 9.) **Two-way Radio Communications Procedures:** Contractor personnel engaged in activities on aircraft movement areas must observe the proper procedures for communications, including using appropriate radio frequencies at airports with and without ATCT. When operating vehicles on or near open runway or taxiways, construction personnel must understand the critical importance of maintaining radio contact, as directed by the Airport Operator. The uniform frequency is 122.7.

- 10.) **Maintenance of the Secured Area of the Airport:** The contractor must take care to maintain security during construction when access points are created or used. The gate at the construction entrance shall be locked except for when entering or exiting.

(6) Wildlife Management

Construction contractors must carefully control and continuously remove waste or loose materials that might attract wildlife. Contractor personnel must be aware of an avoid construction activities that can create wildlife hazards on airports such as:

- a.) **Trash:** Food scraps must be collected from construction personnel activity.
- b.) **Standing Water:** Contractors must minimize the creation of standing water during construction by always maintaining positive drainage. Any standing water that exists after a rainfall event shall be drained immediately. Any pumping required is incidental and at the contractor's expense.
- c.) **Tall Grass and Seeds:** Grass seed is attractive to birds. Lower quality seed mixtures can contain seeds of plants (such as clover) that attract larger wildlife. Seeding shall comply with the project specifications.
- d.) **Poorly Maintained Fencing and Gates:** The Allen County Airport does not currently maintain a security and wildlife fence.
- e.) **Disruption of Existing Wildlife Habitat:** No existing wildlife habitat is expected to be disturbed by this project. The contractor shall notify the airport operator and engineer of wildlife sightings.

(7) Foreign Object Debris (FOD) Management

Waste and loose materials, commonly referred to as FOD, are capable of causing damage to aircraft landing gears, propellers, and jet engines. Construction contractors must not leave or place FOD on or near active aircraft movement areas. Materials capable of creating FOD must be continuously removed during the construction project. Areas open to aircraft operation shall be inspected for FOD by the contractor at the end of each work day if work was done in that area.

(8) Hazard Materials (HAZMAT) Management

Contractors operating construction vehicles and equipment on the airport must be prepared to expeditiously contain and clean-up spills resulting from fuel or hydraulic fluid leaks. Transport and handling of other hazardous materials on an airport also requires special procedures.

(9) Notification of Construction Activities

a.) **List of Responsible Representatives:**

Allen County Airport - Manager
Josh Taltree (419) 227-3225 Office
(567) 208-1871 Mobile

Engineer:
Butler, Fairman & Seufert, Inc. (317) 713-4615 Office

- b.) **NOTAMS:** Only the airport operator or ATCT may initiate or cancel NOTAMs on airport conditions, and is the only entity that can close or open a runway. The airport operator must coordinate the issuance, maintenance and cancellation of NOTAMs about airport conditions resulting from construction activities with tenants and must provide information on closed or hazardous conditions on airport movement areas to the FAA Flight Service Station (FSS) so it can issue a NOTAM. Any person having reason to believe that a NOTAM is missing, incomplete, or inaccurate must notify the airport operator.

c.) **Emergency Notification Procedures:** The following are contacts for the project.

- 1.) Emergencies: Dial 911
2.) Non-Emergency numbers:
Lima Police Department (419) 227-4444
Lima Fire Department (419) 221-5160
Lima Memorial Health System (419) 228-3335
Ohio Regional Poison Control Center (800) 222-1222

- d.) **Coordination with ARFF:** The Allen County Airport does not have Aircraft Rescue and Fire Fighting (ARFF).

e.) **Notification to the FAA:**

- 1.) **Part 77:** FAA Form 7460-1 has been submitted to the FAA for this project. Any deviation from the plan construction areas may require another submission for FAA review.
- 2.) **Part 157:** Title 14 CFR Part 157 does not apply to this project.
- 3.) **NAVAIDS:** For emergency (short-notice) notifications about impacts to both airport owned and FAA owned NAVAIDS, contact (866) 432-5222.

(10) Inspection Requirements

- a.) **Daily Inspections:** Inspections of the site to verify that it is in compliance with the CSPP should be conducted at least daily, but more frequently if needed. The engineer will provide the contractor with a checklist for this inspection. The resident engineer (RE) will also conduct daily inspections.
- b.) **Final Inspections:** Before re-opening closed sections of the airport to operations, the contractor, airport operator, and RE shall inspect the area. Upon their concurrence that the area is safe to operate aircraft, the area will re-open.

(11) Underground Utilities

Locations of all existing underground utilities shown on this plan are based upon above ground evidence (including, but not limited to, manholes, inlets, valves, and marks made upon the ground by others) and are speculative in nature. There may also be other existing underground utilities for which no above ground evidence was observed. The exact locations of said existing underground utilities should be verified by the contractor prior to any and all construction. Ohio Utilities Protection Service may be used to locate the public utilities by calling 811 or (800) 362-2764. They will not however locate airport and FAA owned utilities. Any airport owned utilities that are damaged by construction must be repaired immediately. The contractor shall have an electrician that is available to respond in a timely fashion in case of damage. Known public on-site utilities and their contact information are below:

- a.) **Telephone**
New Knoxville Telephone Company
301 W. South St.
New Knoxville, OH 45871
- b.) **Electric**
AEP Ohio
700 Morrison Road
Gahanna, OH 43230
- c.) **Water**
City of Lima
50 Town Square
Lima, OH 45801
- d.) **Sanitary/Wastewater**
Allen County Sanitary Engineering
3230 N. Cole St.
Lima, OH 45801
- e.) **Fiber**
Centurylink
701 N. Cable Road
Lima, OH 45805
- f.) **F.A.A.**
Charles Edwards
419-408-0760
charles.ledwards@faa.gov

(12) Penalties

Vehicle/Pedestrian Deviations (V/PDs) from the CSPP will face a penalty of varying amount depending on the severity of the deviation. A runway incursion is any unauthorized intrusion onto a runway, regardless of whether or not an aircraft presents a potential conflict. See runway incursion penalty table below:

Runway Incursion Category	Description	Penalty
Category A	A serious incident in which a collision was narrowly avoided.	Up to \$1,500 and Rescission of Access to the AOA.
Category B	An incident in which separation decreases and there is a significant potential for collision, which may result in a time critical corrective/evasive response to avoid a collision.	Up to \$500 and Rescission of Access to the AOA.
Category C	An incident characterized by ample time and/or distance to avoid a collision.	Rescission of driving privileges
Category D	An incident that meets the definition of runway incursion such as incorrect presence of a single vehicle/person/aircraft on the protected area of a surface designated for the landing and take-off of aircraft but with no immediate safety consequences.	Written warning

Violations of the CSPP outside of runway incursions will be assessed a written warning for the first violation and then \$500 per violation thereafter.

(13) Special Conditions

In the case of an aircraft in distress or an accident, all contractor personnel must remove all equipment from the project site and return to the staging area. The project will be suspended until clearance is given from the engineer and the airport operator. In the event of a V/PD the project will be suspended until a safety meeting and de-briefing of the incident occurs.

(14) Runway and Taxiway Visual Aids

Areas where aircraft will be operating are clearly and visibly separated from construction areas, including closed runways. Throughout the duration of the construction project, verify that these areas remain clearly marked and visible at all times and that marking, lighting, signs, and visual NAVAIDS remain in place and operational.

- a.) **General:** Airport markings, lighting, signs, and visual NAVAIDS must be clearly visible to pilots, not misleading, confusing, or deceptive. All must be secured in place to prevent movement by prop wash, jet blast, wing vortices, or other wind currents and constructed of materials that would minimize damage to an aircraft in the event of inadvertent contact.
- b.) **Markings:** Markings must be in compliance with the standards of AC 150/5340-1, Standards for Airport Markings. Runway and exit taxiways closed to aircraft operations are marked with a yellow X.
- 1.) **Closed Runways and Taxiways** (See plan sheets for locations)
- i. **Temporary Closed Runways:** For runways that have been temporarily closed, place an X at each end of the runway directly on or as near as practicable to the runway designation numbers. See lighted closure X detail and notes.
- ii. **Temporary Closed Taxiways:** Place barricades outside the safety area of intersecting taxiways. For runway/taxiway intersections, place an X at the end of the closed taxiway from the runway.
- iii. **Construct the temporary closure X from any of the following materials:** fabric, colored plastic, painted sheets of plywood, snow fence, or similar materials. They must be yellow and properly configured and appropriately secured to prevent movement by prop wash, jet blast, or other wind currents.
- iv. The application rate of paint to mark a short-term temporary runway and taxiway marking may deviate from the standard, but the dimensions must meet the existing standards.
- c.) **Lighting and Visual NAVAIDS:** Lighting must be in conformance with AC 150/5340-30, Design and Installation Details for Airport Visual Aids, and AC 150/5345-50, Specifications for Portable Runway and Taxiway Lights. When disconnecting runway and taxiway lighting fixtures, disconnect the associated isolation transformers. Alternatively, cover the light fixture in such a way as to prevent light leakage. Avoid removing the lamp from energized fixtures because an excessive number of isolation transformers with open secondaries may damage the regulators and/or increase the current above its normal value. Secure, identify, and place any above ground temporary wiring in conduit to prevent electrocution and fire ignition sources.
- 1.) **Temporarily Closed Runways:** The airfield lighting system will be taken out of service when the runway is closed.
- 2.) **Temporarily Closed Runways and Displaced Thresholds:** There will be no partially closed runway during the course of this project.
- 3.) **Temporarily Closed Taxiways:** There will be no partially closed taxiway during the course of this project.
- d.) **Signs:** To the extent possible, signs must be in conformance with AC 150/5345-44, Specifications for Runway and Taxiway Signs and AC 150/5340-18, Standard for Airport Sign Systems. At any time a sign does not serve its normal function; it must be covered or removed to prevent misdirecting pilots.

(15) Marking and Signs for Access Routes

Pavement markings and signs for construction personnel will conform to AC 150/5340-18 and, to the extent practicable, with the Federal Highway Administration Manual on Uniform Traffic Control Devices (MUTCD) and/or State highway specifications.

(16) Hazard Marking and Lighting

Hazard Marking and Lighting prevents pilots from entering areas closed to aircraft, and prevents construction personnel from entering areas open to aircraft.

a.) **Equipment:**

- 1.) **Barricades,** including traffic cones, (weighted or sturdily attached to the surface) are acceptable methods used to identify and define the limits of construction and hazardous areas on the airport. The spacing of barricades must be such that a breach is physically preventable barring a deliberate act. For example, if barricades are intended to exclude vehicles, gaps between barricades must be smaller than the width of excluded vehicles, generally 4 ft.
- 2.) **Lights** must be red, either steady burning or flashing, and must meet the luminance requirements for the State Highway Department. Batteries powering lights will last longer if lights flash. Lights must be mounted on barricades and spaced no more than 10 ft. Lights must be operated between sunset and sunrise and during periods of low visibility whenever the airport is open for operations. They may be operated by photocell, but this may require the contractor to turn them on manually during periods of low-visibility during daytime hours.
- 3.) **Barricades** are not permitted in any active safety area. Within a runway or taxiway object free area, and on aprons, use orange traffic cones, flashing or steady burning red lights as noted above, collapsible barricades marked with diagonal, alternating orange and white stripes; and/or signs to separate all construction/maintenance areas from the movement area. Barricades shall be supplemented with alternating orange and white flags at least 20 by 20 inches square and securely fastened to prevent FOD. All barricades adjacent to any open runway or taxiway/taxiway safety area, or apron, must be as low as possible to the ground, and no more than 18 inches high, exclusive of supplementary lights and flags. Barricades must be of low mass; easily collapsible upon contact with an aircraft or any of its components; and weighted or sturdily attached to the surface to prevent displacement. The airport owns approximately 30 low profile barricades that the contractor may use, however light/flags may need to be provided by the contractor.
- 4.) The contractor shall provide a person on call 24 hours a day for emergency maintenance of airport hazard lighting and barricades. The contractor must file the contact person's information with the airport operator. Lighting should be checked for proper operation frequently.

(17) Protection of Runway and Taxiway Safety Areas

Runway Safety Areas (RSA), Taxiway Safety Areas (TSA), Obstacle Free Zones (OFZ), Object Free Areas (OFA), and approach surfaces must be protected during construction, and are shown on the plan sheets. Protection of these areas includes limitations on the locations and height of equipment and stockpiled material. See the plan sheets of the CSPP for locations and dimensions of the protected areas.

- a.) **Runway Safety Area (RSA) & Taxiway Safety Area (TSA):** A runway safety area is the defined surface surrounding the runway prepared or suitable for reducing the risk of damage to airplanes in the event of an undershoot, overshoot, or excursion from the runway. A taxiway safety area is a defined surface alongside the taxiway prepared or suitable for reducing the risk of damage to an airplane unintentionally departing the taxiway. Construction activities within the existing RSA & TSA are subject to the following conditions:
- 1.) No construction may occur within the existing RSA or TSA while the corresponding runway or taxiway is open for aircraft operations.
- 2.) Open trenches or excavations are not permitted within the RSA or TSA while the corresponding runway or taxiway is open. If possible, backfill trenches before the runway or taxiway is opened. If the runway or taxiway must be opened before the excavations are backfilled, cover the excavations appropriately. Covering for open trenches must allow safe operation of the heaviest aircraft operating on the runway or taxiway across the trench without damage to the aircraft. Construction contractors must prominently mark open trenches and excavations at the site with red or orange flags, as approved by the airport operator, and light them with red lights during hours of restricted visibility or darkness.
- 3.) Soil erosion must be controlled to maintain RSA and TSA standards. The RSA and TSA must be cleared and graded and have no potentially hazardous ruts, humps, depressions, or other surface variations, and capable, under dry conditions of supporting the occasional passage of aircraft without causing structural damage to the aircraft.
- b.) **Runway Object Free Area (ROFA):** Construction, including excavations, may be permitted in the ROFA. However, equipment must be removed from the ROFA when not in use, and material should not be stockpiled in the ROFA if not necessary.
- c.) **Taxiway Object Free Area (TOFA):** Unlike the Runway Object Free Area, aircraft wings regularly penetrate the taxiway object free area during normal operations. Thus, the restrictions are more stringent. Except as noted below, no construction may occur within the TOFA while the taxiway is open for aircraft operations.

- 1.) Construction activity may be accomplished within the TOFA subject to the following restrictions:
- i. Appropriate NOTAMs are issued.
- ii. Marking and lighting meeting the provisions of the CSPP are implemented.
- iii. Five foot clearance is maintained between equipment and materials and any part of an aircraft (includes wingtip overhang). In these situations, flaggers must be used to direct construction equipment, and wing walkers will be necessary to guide aircraft. Wing walkers should be airline/aviation personnel rather than construction workers.
- d.) **Obstacle Free Zone (OFZ):** In general, personnel, material and/or equipment may not penetrate the OFZ while the runway is open for aircraft operations. If a penetration to the OFZ is necessary, it may be possible to continue aircraft operations through operational restrictions.
- e.) **Runway Approach/Departure Areas and Clearways:** All personnel, materials, and/or equipment must remain clear of the applicable threshold sloping surfaces as shown on the plan sheets of the CSPP. Objects that do not penetrate these surfaces may still be obstructions to air navigation.

(18) Other Limitations on Construction


- a.) **Prohibitions:** No use of tall equipment (cranes, concrete pumps, etc.) unless a 7460-1 determination letter is issued for such equipment. No use of open flame welding or torches unless fire safety precautions are provided and the airport operator has approved their use. No use of electrical blasting caps is allowed on or within 1,000 ft. of the airport property. No use of flare pots is allowed within the AOA.
- b.) **Restrictions:** Construction suspension may be required during specific airport operations. The airport operator will notify the contractor and engineer if this is required.

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RECOMMENDED FOR APPROVAL:	DESIGN ENGINEER	DATE
DESIGNED:	BSE	DRAWN: BSE
CHECKED:	JRC	CHECKED: JRC

ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL		HORIZONTAL SCALE	
LIMA		VERTICAL SCALE	
OHIO		N/A	
SHEET		05 OF 11	
PROJECT		AIP No. 3-39-046-023-2021	

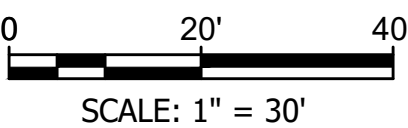
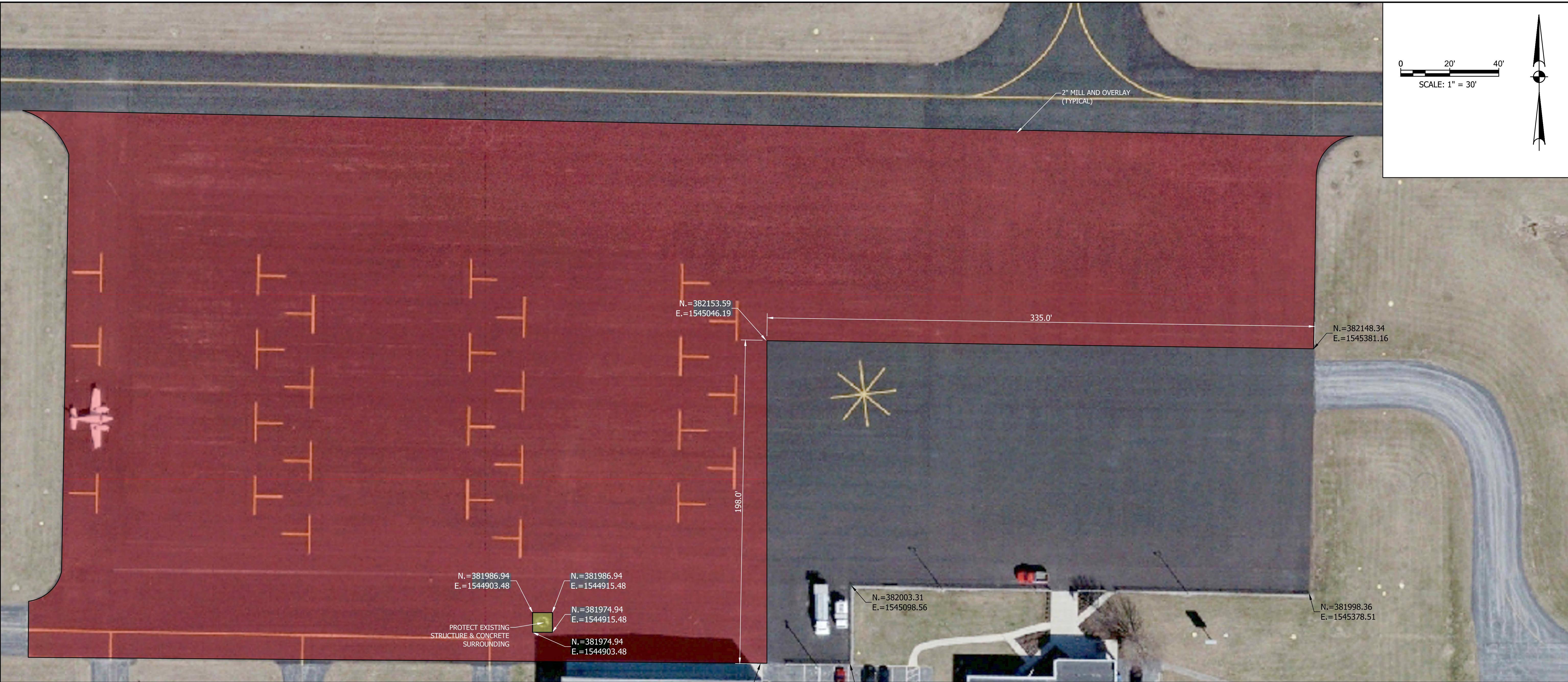
Headquarters
8450 WESTFIELD BLVD., SUITE 300
INDIANAPOLIS, IN 46240-8302
TEL 317-713-4615
FAX 317-713-4616
www.BFSengr.com



Branch Locations
FORT WAYNE 260-459-1532
LOUISVILLE 502-593-1896
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

BFS NO. 6520-9802

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NOTES:

1. APPROXIMATE LOCATIONS OF KNOWN UTILITIES AND CABLES ARE SHOWN. LOCATION OF ALL SURFACE FEATURES, UNDERGROUND UTILITIES, AND STRUCTURES ARE APPROXIMATE AND OBTAINED FROM RECORD DOCUMENTS PROVIDED BY THE ALLEN COUNTY REGIONAL AIRPORT. DEPTHS ARE NOT KNOWN.
2. CONTRACTOR SHALL PROTECT EXISTING AIRCRAFT TIE-DOWNS. IF DAMAGED OCCUR, CONTRACTOR SHALL FIX AT HIS/HER OWN EXPENSE.
3. CONTRACTOR SHALL SAWCUT ALL EXISTING PAVEMENT EDGES FOR MILL AND OVERLAY AREA. CONTRACTOR SHALL PROTECT EXISTING PAVEMENT EDGES.
4. CONTRACTOR SHALL YIELD TO ALL AIRCRAFT ACTIVITIES AT ALL TIMES. CONTRACTOR SHALL NOT PARK ANY EQUIPMENT AND/OR PERSONAL VEHICLES IN FRONT OF ANY HANGARS.
5. CONTRACTOR SHALL PROTECT EXISTING PAINT MARKINGS OUTSIDE OF REHABILITATION LIMITS AT ALL TIMES. IF ANY MARKINGS ARE DAMAGED, CONTRACTOR SHALL REPAINT AT HIS/HER OWN EXPENSE.
6. CONTRACTOR SHALL MAINTAIN DRAINAGE AT ALL TIMES. RESTORE TO ORIGINAL CONDITION ALL DISTURBED AREAS UPON COMPLETION OF PROJECT.


APRON REHABILITATION SCOPE:

1. CONTRACTOR TO PERFORM SAW CUTTING OF PAVEMENT AT REHABILITATION LIMITS.
2. CONTRACTOR TO PERFORM COLD MILLING OPERATION IN ACCORDANCE WITH THE DETAILS.
3. CONTRACTOR TO REPAIR CRACKS IN THE EXISTING ASPHALT AFTER THE MILLING OPERATION IS COMPLETE IN ACCORDANCE WITH THE DETAILS. CONTRACTOR, OWNER AND ENGINEER SHALL EVALUATE AND AGREE ON REPAIR AREAS PRIOR TO COMMENCING.
4. CONTRACTOR SHALL COMPLETE THE ASPHALT SURFACE OVERLAY IN ACCORDANCE WITH THE DETAILS.
5. AFTER APPROXIMATELY 28 DAYS, OR WHEN APPROVED BY THE ENGINEER, THE PAVEMENT MARKINGS ON THE APRON AREA SHALL BE REESTABLISHED PER THE APRON MARKING PLAN SHEET.

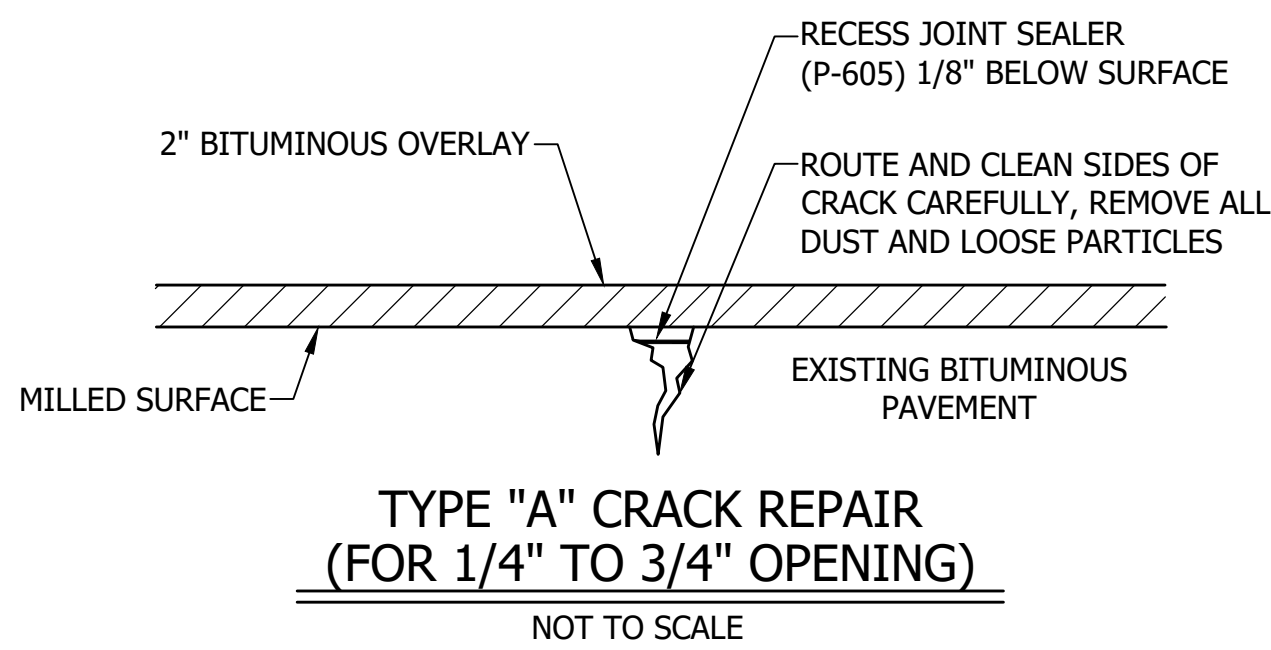
SURVEY CONTROL

COORDINATES PROVIDED IN OHIO STATE PLANE,
OHIO NORTH ZONE, US SURVEY FEET.

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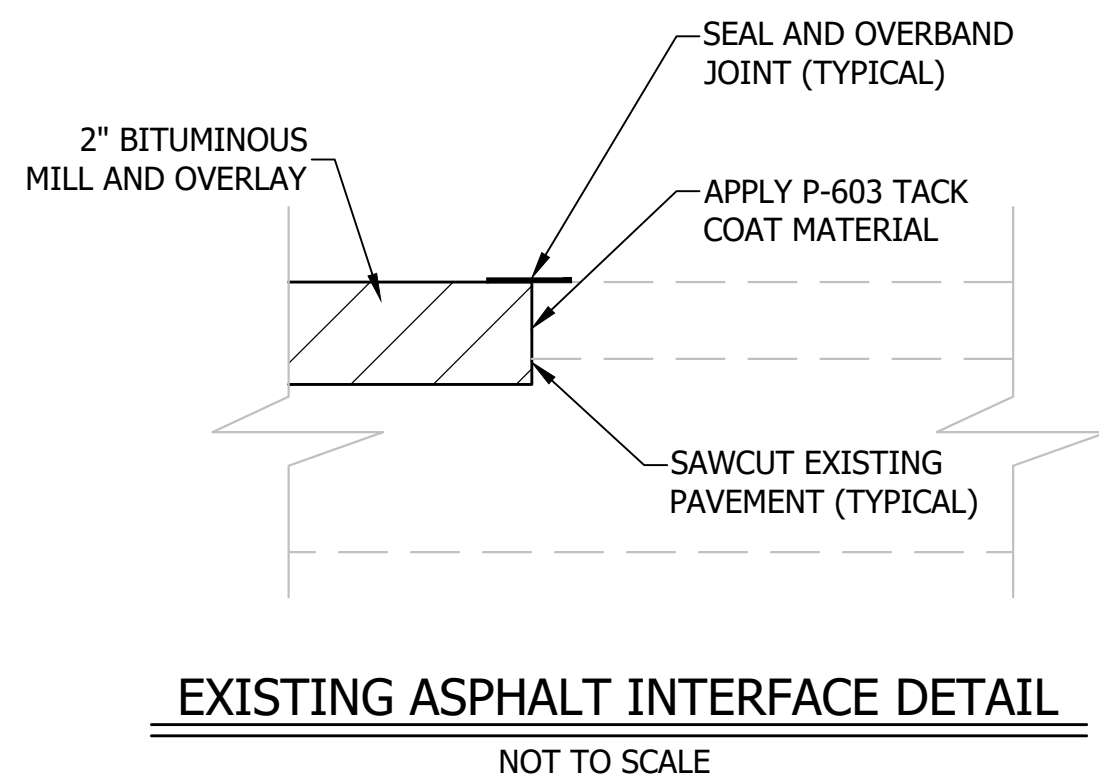
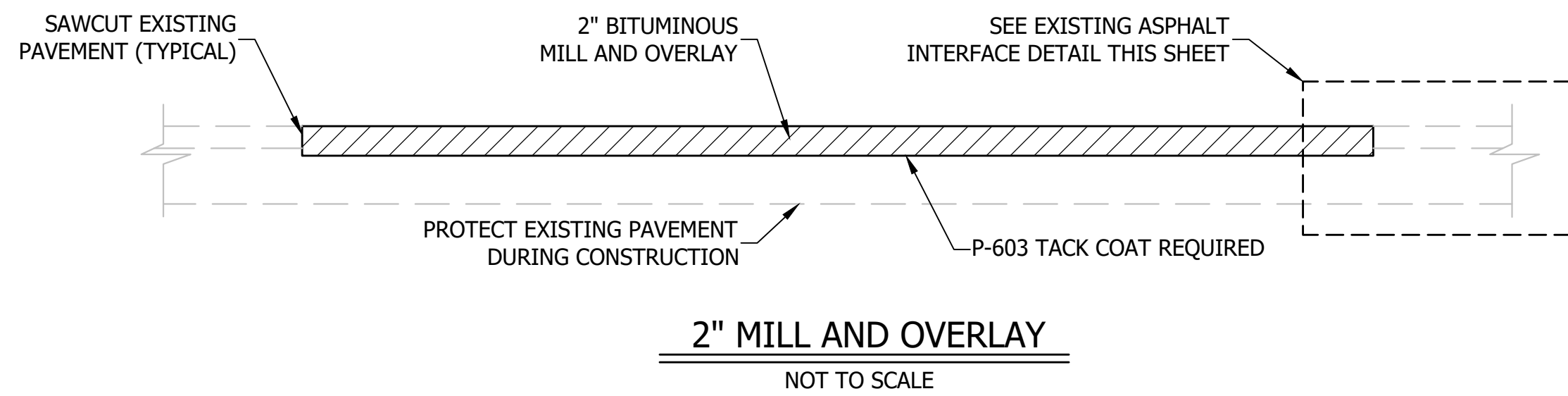
RECOMMENDED FOR APPROVAL: _____ DESIGN ENGINEER DATE 03/24/2021	ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL APRON REHABILITATION PLAN LIMA OHIO	HORIZONTAL SCALE 1" = 20' VERTICAL SCALE N/A	
DESIGNED: BSE DRAWN: BSE	Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.bfsengr.com	 Butler Faernham Seufert ENGINEERS	Branch Locations FORT WAYNE 260-459-1532 LOUISVILLE 502-593-1996 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242
CHECKED: JRC CHECKED: JRC	SHEET 06 OF 11 PROJECT AIP No. 3-39-046-023-2021	BFS NO. 6520.9802	

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CRACK REPAIR NOTES

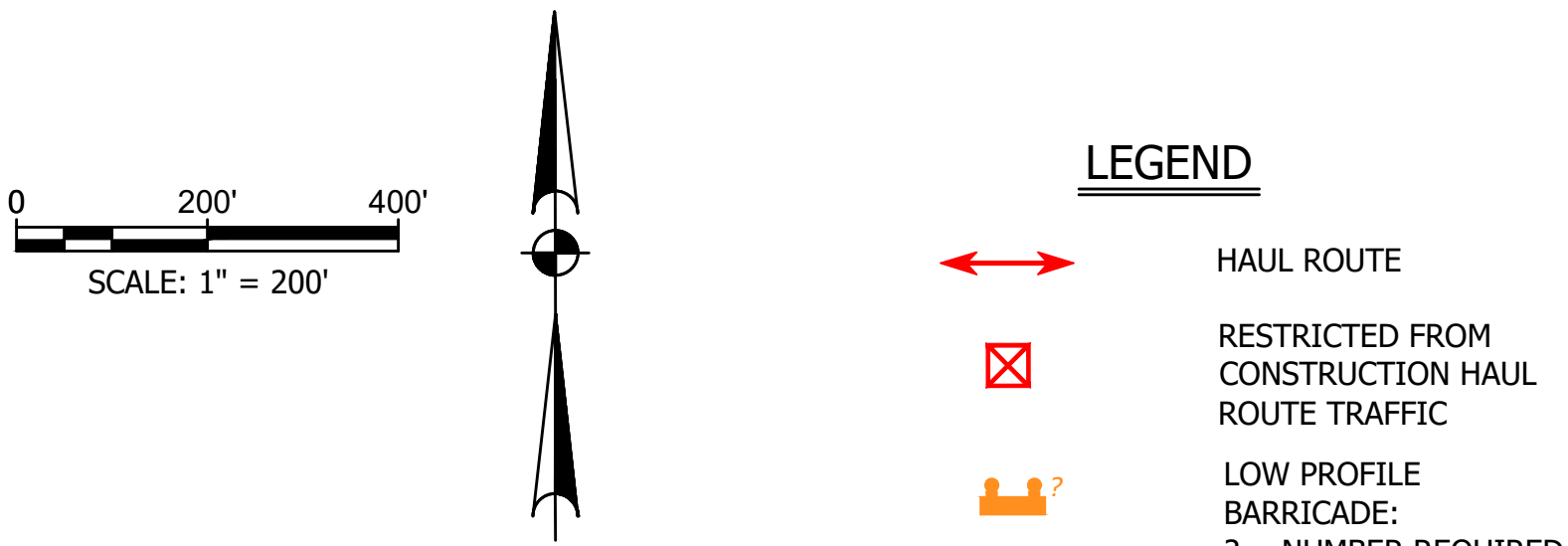
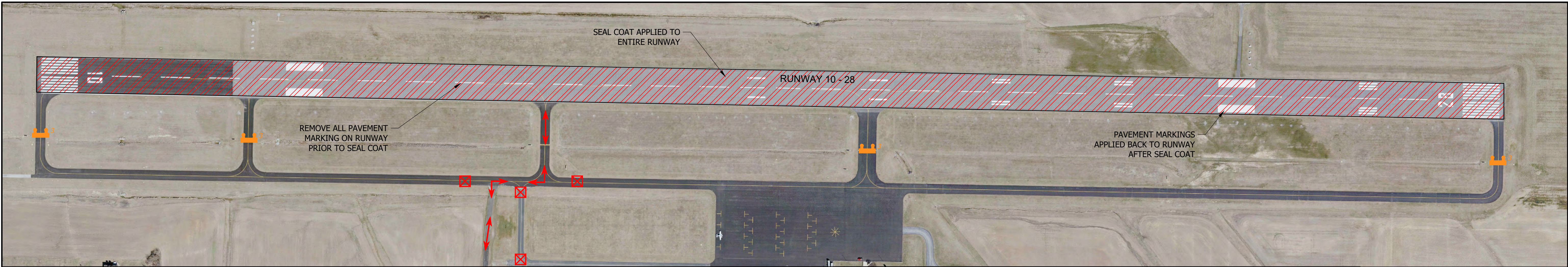
1. REMOVE ALL DAMAGED MATERIAL IN PAVEMENT AREAS.
2. TYPE "A" CRACK REPAIRS NOT SHOWN FOR CLARITY. LOCATIONS AND LENGTH TO BE DETERMINED BY FIELD ENGINEER.
3. FIELD ENGINEER MAY ADD, SUBTRACT, OR ADJUST LOCATION AND LENGTH OF REPAIRS, OR REPAIR TYPE, AT HIS/HER DISCRETION.
4. AIR BLAST CLEAN CRACK AND EXPOSED SURFACE FULL DEPTH OF CRACK IF POSSIBLE. MINIMUM CLEANING DEPTH 3".
5. ALL MATERIAL, EQUIPMENT & LABOR REQUIRED TO COMPLETE CRACK REPAIR TO BE INCLUDED IN COST OF EACH INDIVIDUAL TYPE OF CRACK REPAIR, PER LINEAR FOOT.



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<div>90% DRAFT 03/24/2021</div>	RECOMMENDED FOR APPROVAL: _____ DESIGN ENGINEER _____ DATE <u>03/24/2021</u>	ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL APRON REHABILITATION DETAILS LIMA OHIO	HORIZONTAL SCALE VERTICAL SCALE N/A
	DESIGNED: _____ BSE DRAWN: _____ BSE	Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSengr.com	Branch Locations FORT WAYNE 260-459-1532 LOUISVILLE 502-593-1996 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242
	CHECKED: _____ JRC CHECKED: _____ JRC	BFS Butler Fairman Seufert ENGINEERS	SHEET 07 OF 11 PROJECT AIP No. 3-39-046-023-2021

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GENERAL NOTES:

1. CONTRACTOR SHALL MAINTAIN DRAINAGE AT ALL TIMES. RESTORE TO ORIGINAL CONDITION ALL DISTURBED AREAS UPON COMPLETION OF PROJECT.
2. CONTRACTOR SHALL PROTECT EXISTING TAXIWAY PAINT MARKINGS OUTSIDE OF SEAL COAT LIMITS AT ALL TIMES. IF ANY MARKINGS ARE DAMAGED, CONTRACTOR SHALL REPAINT AT HIS/HER OWN EXPENSE.
3. PRIOR TO REOPENING A SITE INSPECTION SHALL BE CONDUCTED BY THE AIRPORT TO ENSURE THAT ALL AIRPORT SURFACES ARE CLEAN OF ANY DEBRIS.
4. KEEP PAVEMENT USED FOR HAULING CLEAN AT ALL TIMES, EVEN DURING HAULING OPERATIONS TO AND FROM SITE.
5. HAVE VAC TRUCK AVAILABLE AT ALL TIMES DURING OPERATIONS REQUIRING CONSTRUCTION TRAFFIC TO HAUL TO AND FROM SITE TO CLEAN FOREIGN OBJECT DEBRIS (FOD) FROM AIRCRAFT PAVEMENT IMMEDIATELY.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR IDENTIFYING ALL BELOW GROUND UTILITIES, INCLUDING AIRPORT OWNED, WITHIN CONSTRUCTION LIMITS AND ALL HAUL ROUTES.
7. CONTRACTOR SHALL USE HAUL ROUTE AS SHOWN. CONTRACTOR SHALL YIELD TO ALL AIRCRAFT ACTIVITIES AT ALL TIMES. CONTRACTOR SHALL NOT PARK ANY EQUIPMENT AND/OR PERSONAL VEHICLES IN FRONT OF ANY HANGARS AND ON OR NEAR ANY TIE DOWN MARKINGS.

RUNWAY SEAL COATING SCOPE:

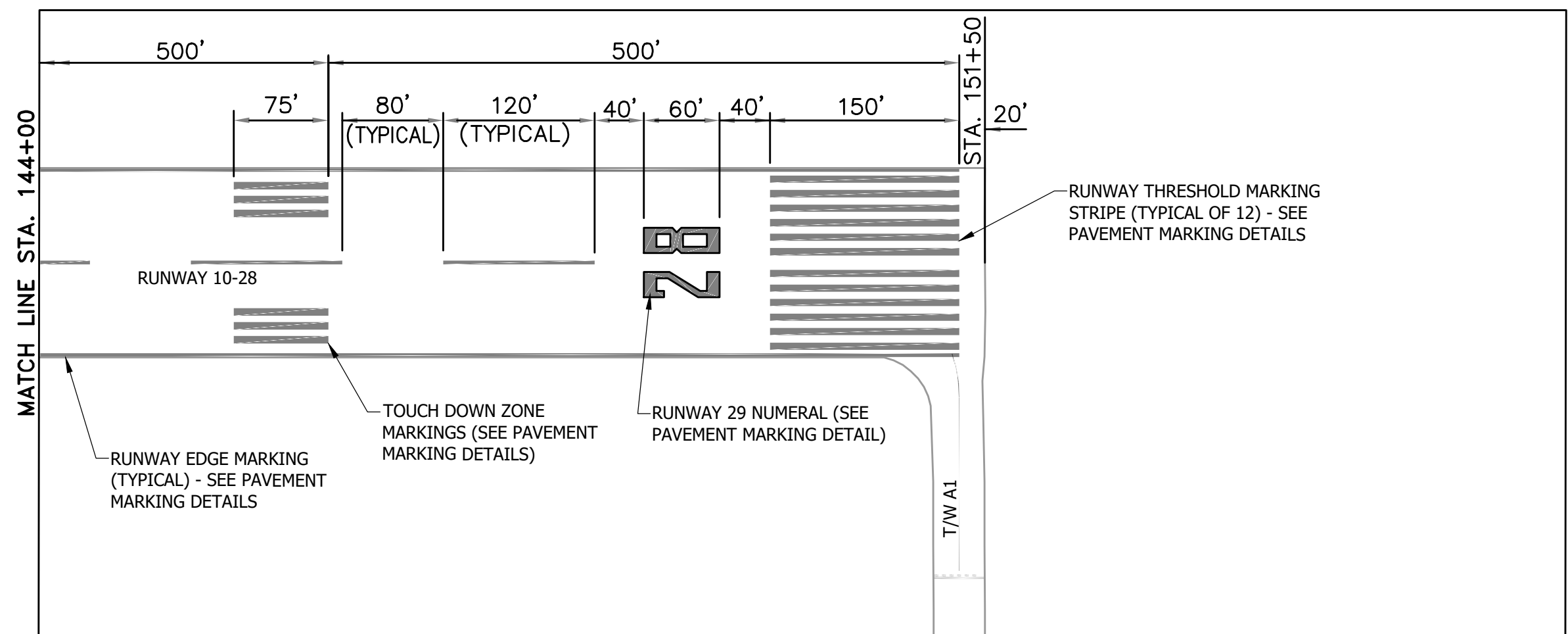
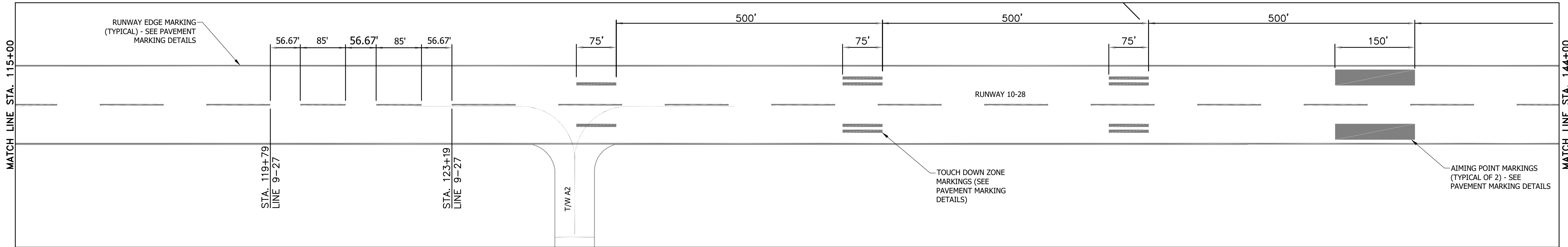
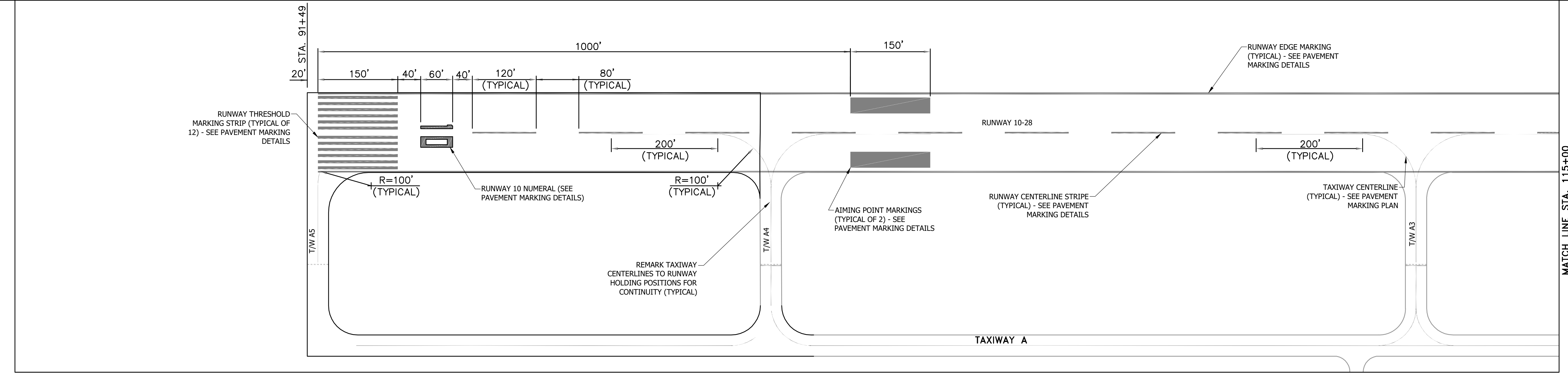
1. CONTRACTOR TO REMOVE EXISTING PAVEMENT MARKINGS WITH WATER BLASTING METHODS.
2. CONTRACTOR TO REPAIR CRACKS IN THE EXISTING ASPHALT IN ACCORDANCE WITH THE DETAILS. CONTRACTOR, OWNER AND ENGINEER SHALL EVALUATE AND AGREE ON REPAIR AREAS PRIOR TO COMMENCING.
3. CONTRACTOR SHALL APPLY THE P-608 SEAL COAT MATERIAL IN ACCORDANCE WITH THE SPECIFICATIONS AND MANUFACTURERS RECOMMENDATIONS.
4. TEMPORARY PAVEMENT MARKINGS SHALL BE APPLIED AS SOON AS THE SEAL COAT MATERIAL ALLOWS AND THE RUNWAY SHALL BE RE-OPENED TO TRAFFIC.
5. AFTER APPROXIMATELY 28 DAYS, OR WHEN APPROVED BY THE ENGINEER, THE PERMANENT PAVEMENT MARKINGS ON THE RUNWAY SHALL BE APPLIED IN ACCORDANCE TO THE SPECIFICATIONS.

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<div>90% DRAFT 03/24/2021</div>	RECOMMENDED FOR APPROVAL: _____ DESIGN ENGINEER _____ DATE <u>03/24/2021</u>	ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL RUNWAY SEALING PLAN		HORIZONTAL SCALE 1" = 200'
	DESIGNED: BSE DRAWN: BSE	LIMA	OHIO	VERTICAL SCALE N/A
	CHECKED: JRC CHECKED: JRC	Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSengr.com		Branch Locations FORT WAYNE 260-459-1532 LOUISVILLE 502-593-1996 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242
	BFS Butler Fairman Seufert ENGINEERS		SHEET 08 OF 11 PROJECT AIP No. 3-39-046-023-2021	

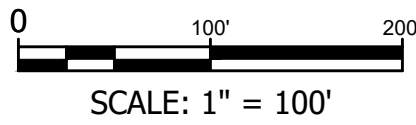
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PAVEMENT MARKING NOTES:

- ALL TAXIWAY CENTERLINE MARKINGS ARE TO BE INTERRUPTED FOR RUNWAY MARKINGS.
- ALL RUNWAY AND TAXIWAY LIGHT FIXTURES AND GUIDANCE SIGNS SHALL BE PROTECTED FROM OVERSPRAY.
- GLASS BEADS SHALL BE APPLIED TO THE PAINT AT THE RATE DESIGNATED IN THE SPECIFICATIONS.
- IMMEDIATELY PRIOR TO THE APPLICATION OF PAINT ALL SURFACES SHALL BE DRY AND FREE FROM DIRT, GREASE, OIL, LAITANCE, OR OTHER FOREIGN MATERIAL WHICH WOULD REDUCE THE BOND BETWEEN THE PAINT AND THE PAVEMENT. THIS SHALL INCLUDE EXISTING PAINTED AREAS.
- RUNWAY CENTERLINE MARKING AND ALL PAVEMENT MARKINGS ON NEW PAVEMENTS SHALL BE APPLIED AT A 50% APPLICATION RATE WITHOUT GLASS BEADS. AFTER 30 DAY CURE TIME FOR NEW PAVEMENT ALL PAVEMENT MARKINGS SHALL BE APPLIED AT A 100% APPLICATION RATE WITH GLASS BEADS AS SHOWN IN THIS PLAN. PAYMENT SHALL BE MADE UNDER ITEM P-620 PAVEMENT MARKINGS.
- ANY PAVEMENT MARKINGS APPLIED IN ERROR SHALL BE REMOVED BY APPROVED METHODS AT THE CONTRACTOR'S EXPENSE. APPLICATION OF BLACK OR GRAY PAINT WILL NOT BE ALLOWED.



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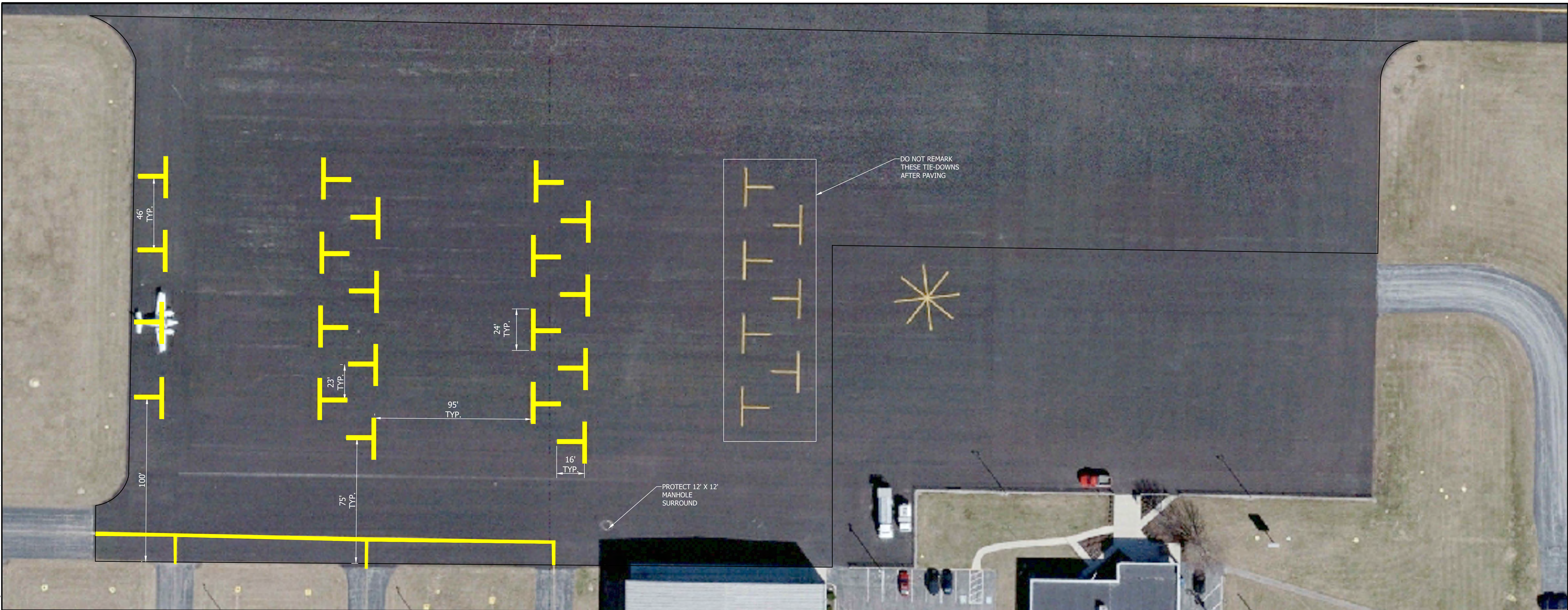
ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL	
RUNWAY MARKING PLAN	
LIMA	OHIO
Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSengr.com	
Branch Locations FORT WAYNE 260-459-1532 LOUISVILLE 502-593-1996 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-639-3242	



HORIZONTAL SCALE	
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VERTICAL SCALE	
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NOTES:

1. APPROXIMATE LOCATIONS OF KNOWN UTILITIES AND CABLES ARE SHOWN. LOCATION OF ALL SURFACE FEATURES, UNDERGROUND UTILITIES, AND STRUCTURES ARE APPROXIMATE AND OBTAINED FROM RECORD DOCUMENTS PROVIDED BY THE ALLEN COUNTY REGIONAL AIRPORT. DEPTHS ARE NOT KNOWN.
2. CONTRACTOR SHALL PROTECT EXISTING AIRCRAFT TIE-DOWNS. IF DAMAGED OCCUR, CONTRACTOR SHALL FIX AT HIS/HER OWN EXPENSE.
3. CONTRACTOR SHALL YIELD TO ALL AIRCRAFT ACTIVITIES AT ALL TIMES. CONTRACTOR SHALL NOT PARK ANY EQUIPMENT AND/OR PERSONAL VEHICLES IN FRONT OF ANY HANGARS.
4. CONTRACTOR SHALL PROTECT EXISTING PAINT MARKINGS OUTSIDE OF REHABILITATION LIMITS AT ALL TIMES. IF ANY MARKINGS ARE DAMAGED, CONTRACTOR SHALL REPAINT AT HIS/HER OWN EXPENSE.
5. CONTRACTOR SHALL MAINTAIN DRAINAGE AT ALL TIMES. RESTORE TO ORIGINAL CONDITION ALL DISTURBED AREAS UPON COMPLETION OF PROJECT.

0 30' 60'
SCALE: 1" = 30'

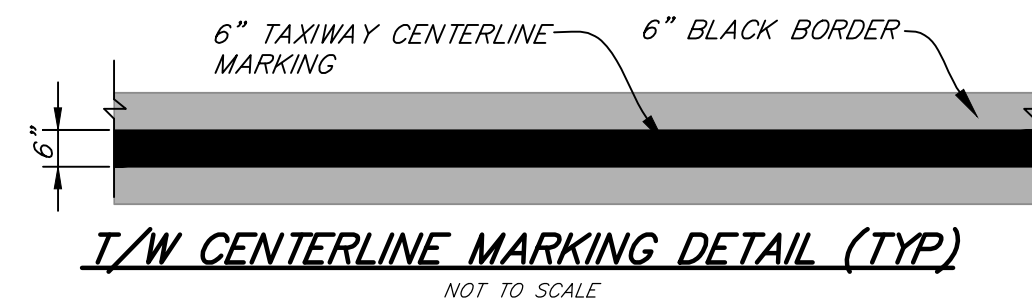
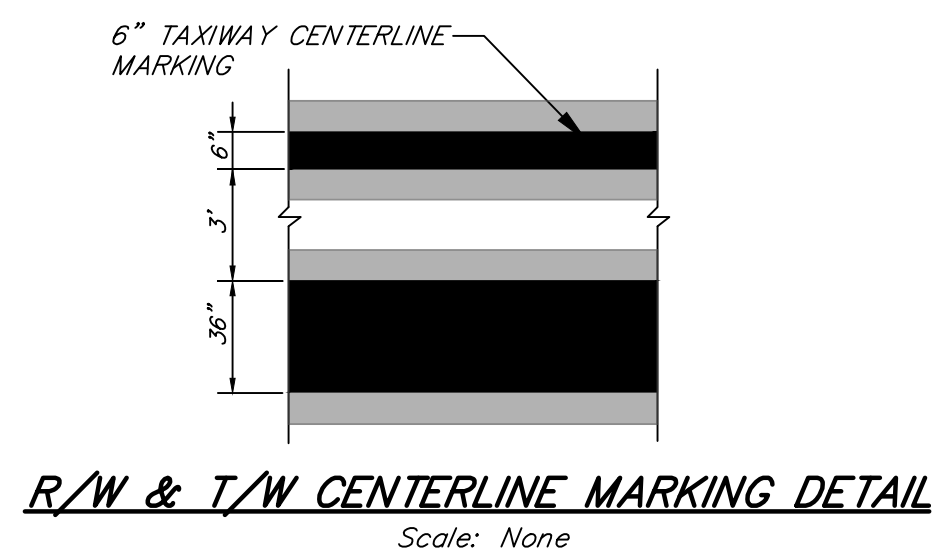
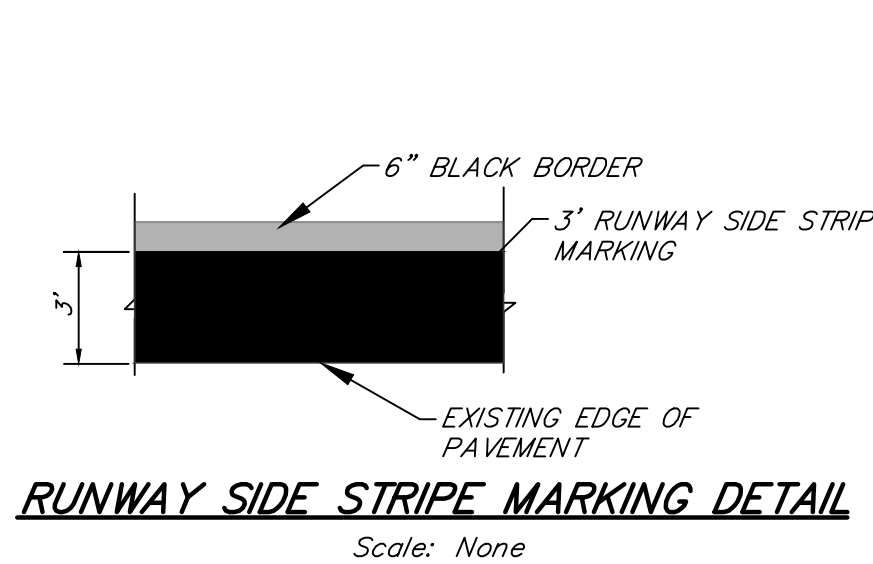
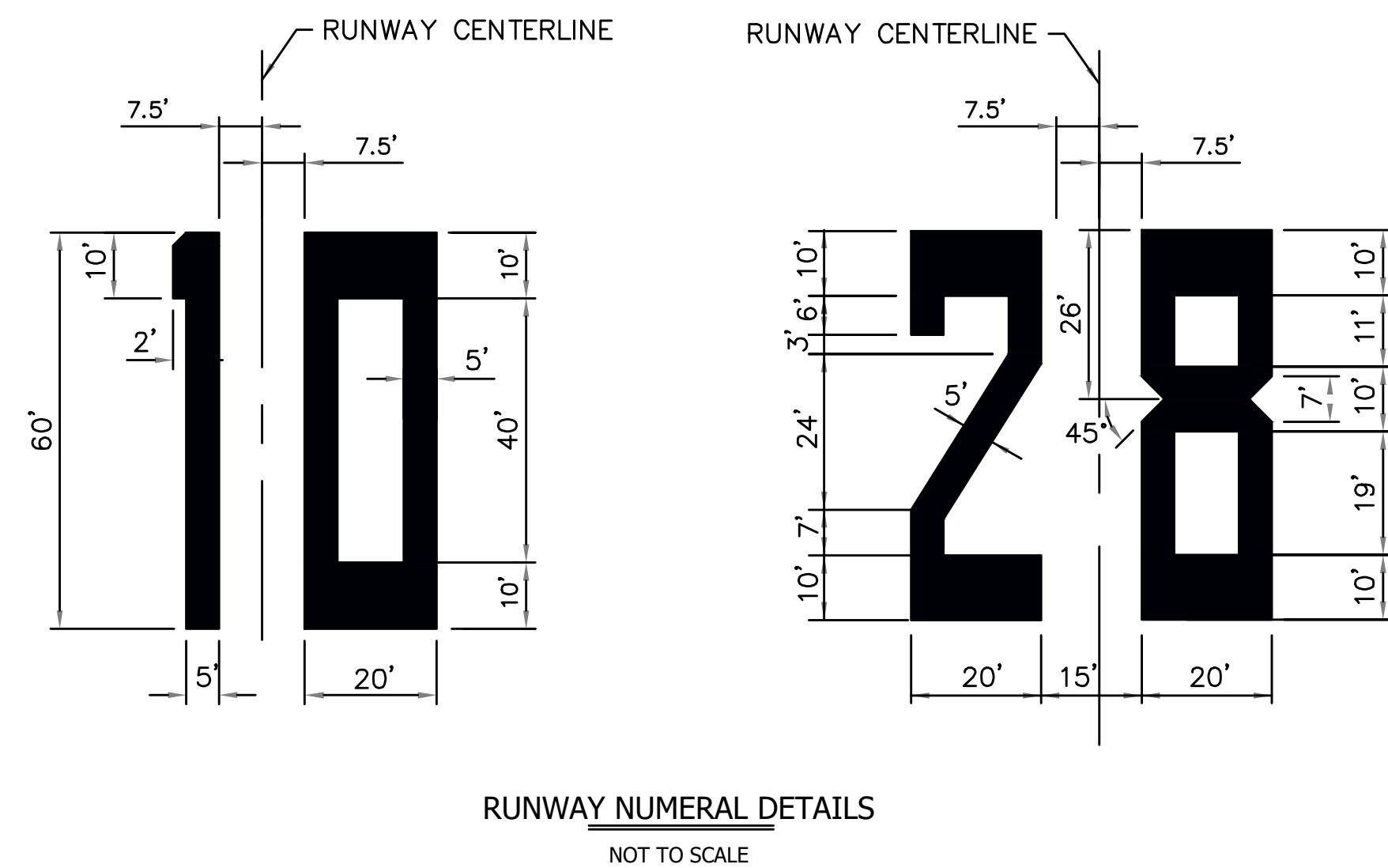
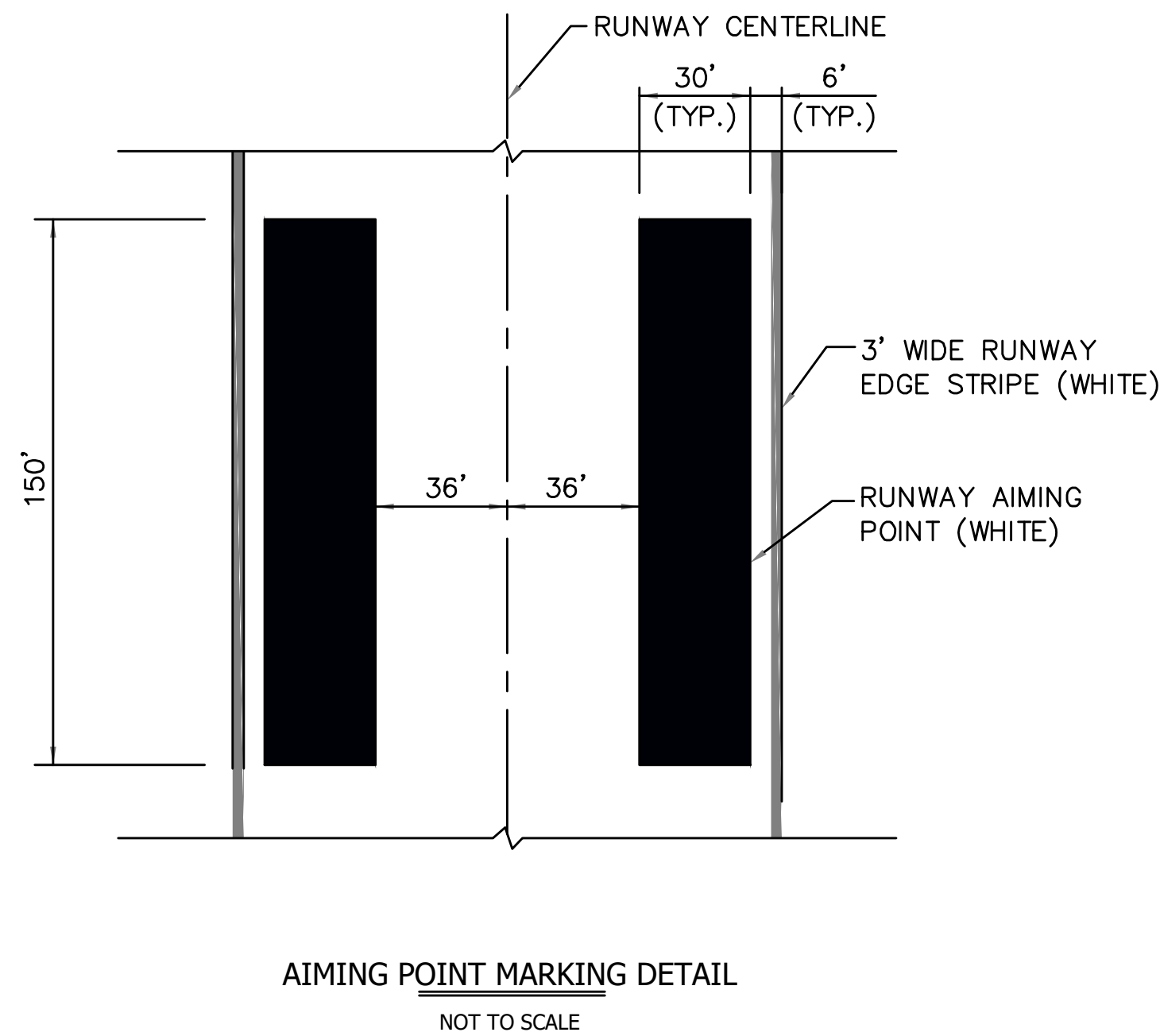
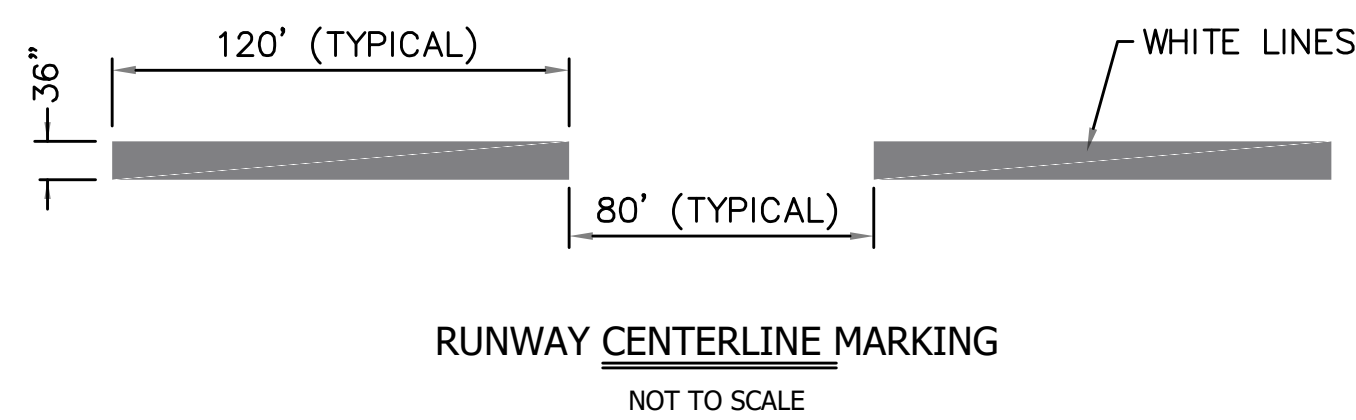
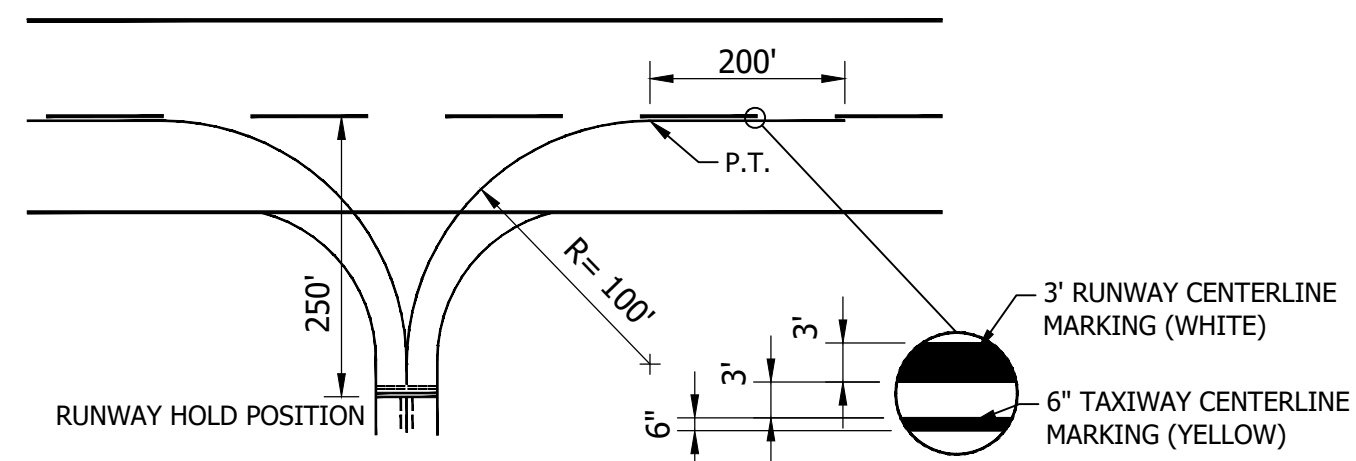
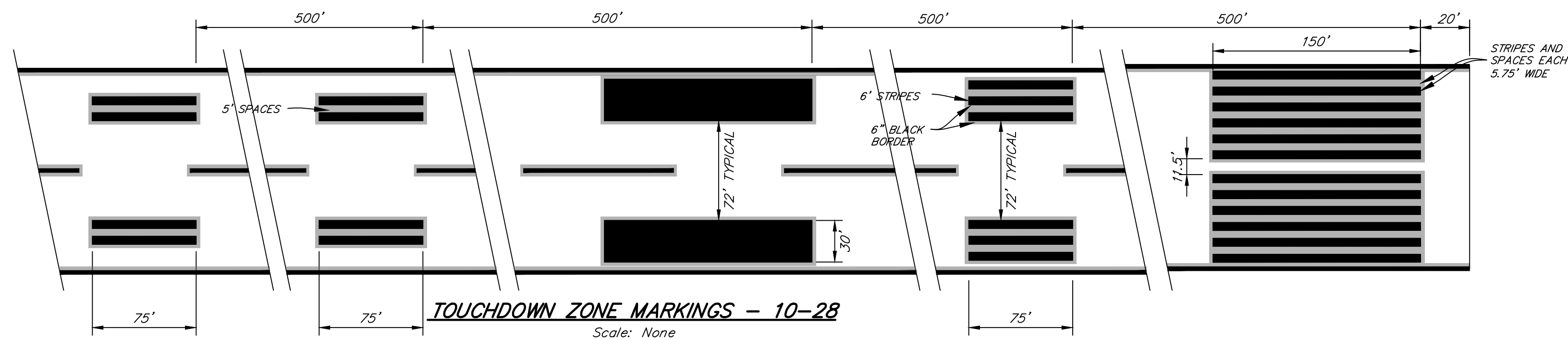


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RECOMMENDED FOR APPROVAL: <u>03/24/2021</u> DESIGN ENGINEER DATE	ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL APRON MARKING PLAN OHIO	HORIZONTAL SCALE 1" = 200'
DESIGNED: BSE DRAWN: BSE	LIMA	VERTICAL SCALE N/A
CHECKED: JRC CHECKED: JRC	Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSengr.com	Branch Locations FORT WAYNE 260-459-1532 LOUISVILLE 502-593-1996 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242
		SHEET 10 OF 11
		PROJECT AIP No. 3-39-046-023-2021

BFS NO. 6520.9802

\\bfsc1241\jobs\1652000.9802\ProjDevelopment\Design Drawings\6520-DET1-PAVEMENT-MARKINGS.dwg Jason Clearwaters Plot:3/24/2021 7:11 PM Save:3/24/2021 4:48 PM



PAVEMENT MARKING NOTES:

- COLORS:
 - WHITE - DESIGNATION MARKINGS, RUNWAY CENTERLINE STRIPES, RUNWAY EDGE STRIP, AIMING POINT MARKING, THRESHOLD MARKINGS, AND TOUCHDOWN ZONE MARKING.
 - YELLOW - TAXIWAY CENTERLINE STRIPES
- ALL MARKINGS REQUIRE A 6" BLACK BORDER.
- PAINT REMOVAL SHALL BE COMPLETED USING A COMBINATION OF A CHEMICAL PAINT REMOVER AND WATER BLASTING. PAINT REMOVAL BY GRINDING OR OTHER METHODS WHICH DAMAGE THE PAVEMENT SURFACES SHALL NOT BE USED.
- ALL RUNWAY AND TAXIWAY LIGHT FIXTURES SHALL BE PROTECTED FROM OVERSPRAY.
- ALL MARKINGS SHALL CONFORM WITH THE LATEST ADDITION OF ADVISORY CIRCULAR 150/5340-1.
- ALL PAINT SHALL INCLUDE REFLECTIVE MEDIA, EXCEPT FOR BLACK BORDER.
- ALL PAINT SHALL CONFORM TO ITEM P-620 OF THE SPECIFICATIONS.
- ALL TAXIWAY CENTERLINE MARKINGS ARE TO BE INTERRUPTED FOR RUNWAY MARKINGS.

REVISIONS			
NO.	REMARK	DATE	BY
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RECOMMENDED FOR APPROVAL: DESIGN ENGINEER DATE 03/24/2021	ALLEN COUNTY REGIONAL AIRPORT - APRON REHAB & RUNWAY SEAL RUNWAY MARKING DETAILS LIMA OHIO	HORIZONTAL SCALE 1" = 200' VERTICAL SCALE N/A
DESIGNED: BSE DRAWN: BSE	HEADQUARTERS 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.bfsengr.com	BRANCH LOCATIONS FORT WAYNE 260-459-1532 LOUISVILLE 502-593-1996 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-639-3242
CHECKED: JRC CHECKED: JRC	SHEET 11 OF 11 PROJECT AIP No. 3-39-046-023-2021	BFS NO. 6520.9802